



BALDWIN PARK TDRC

APRIL 24, 2008

MPL2008-00010

Applicant

Hans Clemmensen, Post Apartments Homes, LP

Project Location

Lots 206 & 207 on the southwest side of Common Way Road, between New Broad Street and Lake Baldwin Lane (±1.64 acres)

Project Description

1. Request for Specific Parcel Master Plan approval for a 30 unit multifamily development and Leasing Center as follows: Lot 206—two freestanding residential buildings with surface parking and accessory garages, and Lot 207—two freestanding buildings connected by a covered walkway with surface parking and accessory garages.

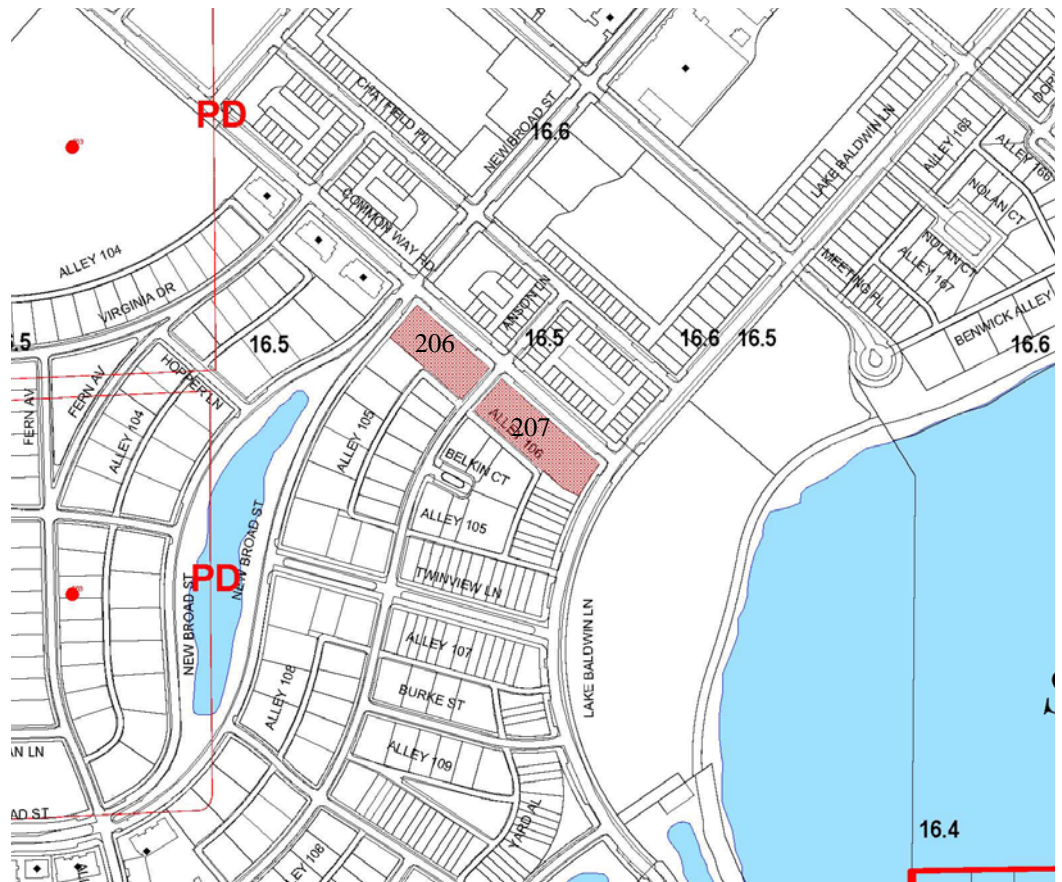
Staff Recommendation

Approval, subject to the conditions in this staff report.

Project Planner

Holly Stenger, AIA

UNIT 1 LOTS 206 & 207—POST LAKE AT BALDWIN PARK PHASE IV



Location Map: Unit 1—
Neighborhood General

FIGURE 1: HISTORY

1. PD Ordinance—Approved July 27, 1998
2. Infrastructure Master Plan (MPL2000-00001) - approved December 15, 2000
3. Westside Neighborhood Master Plan (MPL2001-00031) Approved March 22, 2001
4. Landscape Master Plan (MPL2001-00005) Approved April 26, 2001
5. Lake Baldwin Lane (ENG2004-01434)
6. Lake Baldwin Lane Preliminary Plat (SUB 2004-00015)
7. Application Lots 206 & 207 SPMP February 26, 2008.
8. Both lots have previously been used as parking lots for the Baldwin Park Model Homes and are currently landscaped to meet the parking lot requirements.
9. Project was deferred at the March 31st TDRC meeting.



Project Description

The applicant is requesting Specific Parcel Master Plan approval for a 30 unit multi-family development within 3 free-standing buildings on Lots 206 and 207 in Baldwin Park Unit 1. Both lots are located on the south side of Common Way Road, between New Broad street and Baldwin Park Lane. Anson Lane is located between Lots 206 and 207. Both lots are located within the Neighborhood General designation on the Regulatory Plan.

- All 30 units will be apartment rental units
- The ground floor of the corner building on Lot 206 will be the Leasing Center for units on Lots 206 and 207 and for the 380 residential rental units proposed along the west side of Lake Susannah (MPL2008-00009).
- Density/Intensity—Total lot area is 1.64 acres. 30 units are proposed for a total of 18.29 units per acre.
- Total proposed residential parking spaces: 43, including 38 on-site surface spaces, 2 H.C. spaces, and 3 garage spaces. All on-site parking is located behind the buildings. The Code requires 49 spaces for residential and 10 spaces for the office use. There are 24 on-street parking spaces directly adjacent to the property, which the applicant would like to include for the required parking.

Consistency with the Baldwin Park PD

The Neighborhood General Development standards encourage primary orientation toward the street and parks. Buildings shall create a street edge and garages or parking shall be located behind the buildings or orient toward the alleys. Sidewalks shall be continuous and be on both sides of the street.

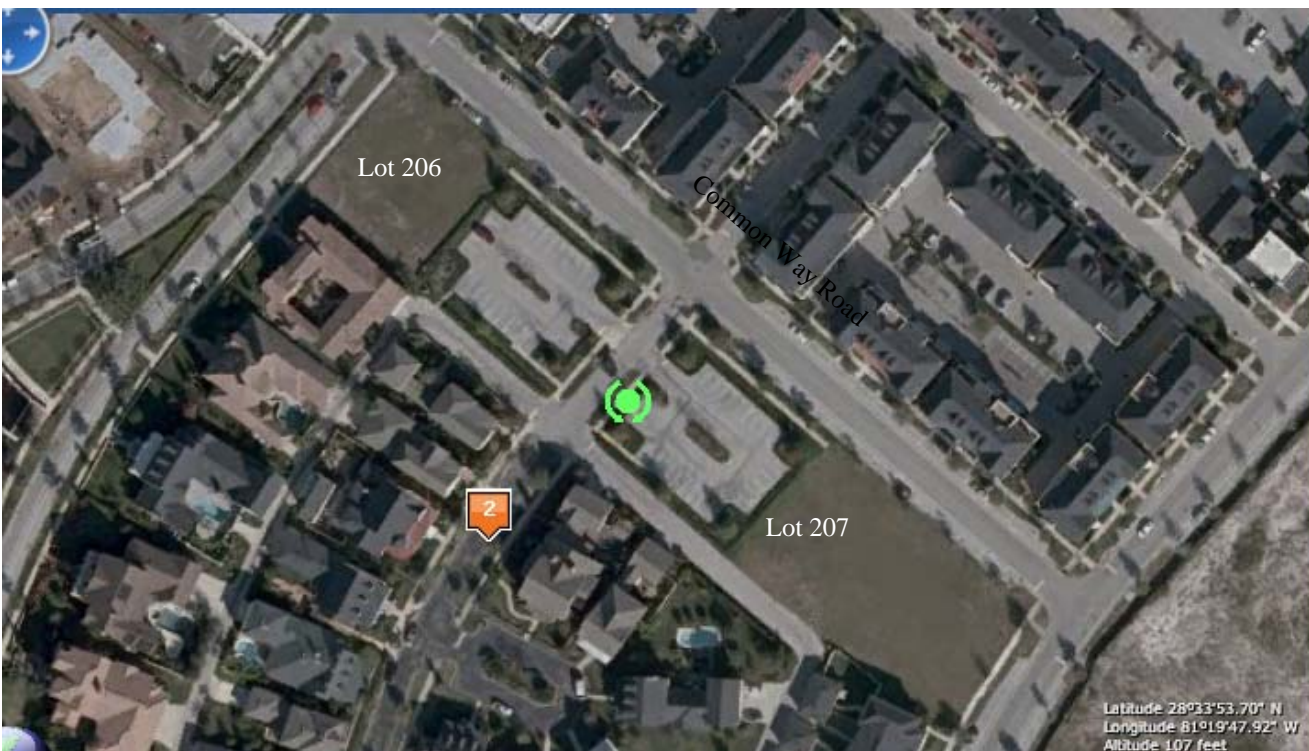
Development Standards for Attached Housing:

- Maximum block size: 200' - 300' width x 300' - 500' length

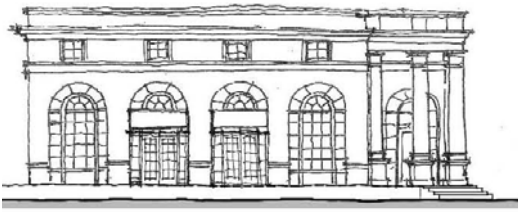
- Maximum building width—200'
- Setbacks—12' minimum front setback—build to line.
- 5' - minimum setback from the street ROW for all parking lots
- Balconies, porches, stoops, roof overhangs may encroach within setback area
- Frontages—a minimum of 60% of block width must be building frontage; a minimum of 80% of buildings on the block should conform to the build-to line
- Parking lots shall be screened from streets with a 2' to 4' high wall or hedge.
- Building Height—maximum height is 3 stories; building heights are encouraged to step up at block corners; entry level to be located a minimum 18" above the sidewalk.
- Parking—on street parking will occur on all neighborhood streets; off-street parking for each residential unit will be provided; all off-street parking will be located behind buildings.

The proposed buildings on Lot 206 address the corner and are located adjacent to the street. The parking area between the buildings is set back from the front of the buildings and shall be screened with a low wall or hedge. The accessory garage structure also continues the street edge along the secondary street (Anson Lane).

The proposed buildings on Lot 207 are broken up into three masses, but the two largest buildings are connected by a cat-walk along the rear. The accessory garage along Lake Baldwin Lane continues the building edge, and shall incorporate transparency along the principal façade. The buildings have entry porches that encroach into the setback. The principal facades shall meet the minimum 5 ft. setback, unless otherwise approved by the TDRC, or through review by the Town Architect and Town Planner.



Current condition with paved parking lots for model homes and open areas.



1 NORTH ELEVATION
SCALE: 1/8" = 1'-0"

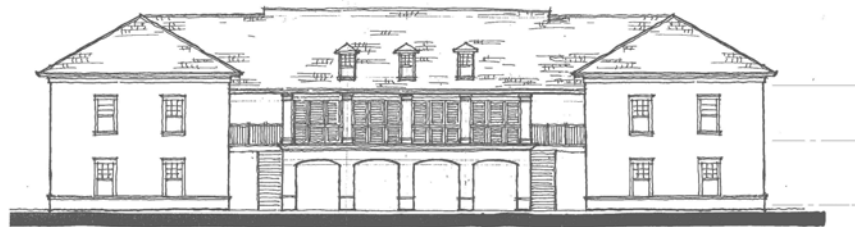


2 WEST ELEVATION
SCALE: 1/8" = 1'-0"

Lot 206: Proposed elevations of the Leasing Center. North elevation faces Common Way Road, and West elevation is along New Broad Street. Notice the grade change from Common Way road to the rear of the property. (Not to scale)

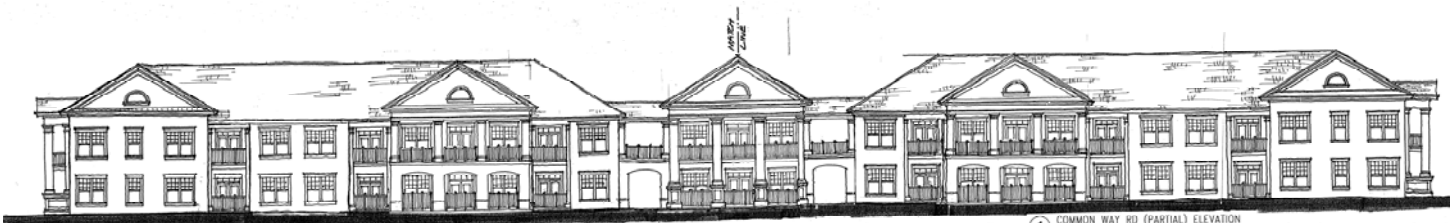


1 COMMON WAY ROAD ELEVATION
SCALE: 1/8" = 1'-0"



2 PARKING LOT ELEVATION
SCALE: 1/8" = 1'-0"

Lot 206: Proposed Elevations of the second building on Lot 206. (Not to scale)



1 COMMON WAY RD (PARTIAL) ELEVATION
SCALE: 1/8" = 1'-0"

1 COMMON WAY RD (PARTIAL) ELEVATION
SCALE: 1/8" = 1'-0"

Lot 207: Proposed front elevations of the building on Lot 207. The building is broken into three masses, though all will be connected by a covered walkway at the rear of the building. (Not to scale)



Lot 207: Photo looking west across Common Way Road. The grade change is visible from the front of the property to the rear. Sidewalks shall be installed and continue from New Broad Street to Lake Baldwin Lane.



Existing parking lots for the model homes. The surrounding buildings vary from single family homes to 3-story multi-family buildings.

Vehicular circulation and Parking

The proposed development fits within the existing street network. No new access points are proposed. The majority of the parking is surface parking behind the buildings. Additional parking is provided between buildings or within accessory garage structures. Parking areas that are located between buildings are shown with low walls and shall be screened from the view of the ROW. All existing on-street parking will remain, however, 10 on-street parking spaces adjacent to Lot 206 are proposed to be used for required parking for the Leasing Office use, and 9 spaces adjacent to Lot 207 are proposed to be used for required parking for residential use.

Summary

Staff recommends that TDRC approve the Specific Parcel Master Plan, subject to the conditions contained in this staff report. As indicated in the conditions, several conditions may affect the final site plan, including the amount of on-site surface parking, the size and location of handicap parking, and the location of accessory parking garages.



Existing buildings along Lake Baldwin Lane and Common Way intersection.

CONDITIONS OF APPROVAL

1. Final site plan and building elevations shall be reviewed and approved by the Town Architect and Town Planner prior to submitting for building permits.
2. A landscape plan was not submitted with the application. Landscape plans shall be submitted for review and approval by the Town Architect and Town Planner prior to submitting plans for permit approval.
3. A min. 5-foot wide landscape buffer yard shall be installed along the south property lines of both lots to screen the parking areas along the alley ways.
4. Sidewalks shall be installed or continued along New Broad Street, Common Way Road, Anson Lane, and Lake Baldwin Lane. If sidewalks are damaged during construction, applicant shall replace damaged sidewalk.
5. Parking: The proposed development on Lot 207 is deficient 9 parking spaces. On-street parking adjacent to site may be counted toward required residential parking. Office and visitor parking may also be on-street parking.
6. All enclosed living space, on all floors, shall be behind the setback line.
7. Accessory garage structures shall incorporate 15% transparency on facades facing the street.
8. All buildings shall incorporate an entrance on facades facing the street. Pedestrian connections from the entrances to the public sidewalks shall be required. Individual entrances to each ground floor unit are encouraged.

Transportation Conditions:

For questions regarding this Transportation Department TRC application review, please contact Adam Walosik at (407) 246-3528 or Adam.Walosik@cityoforlando.net

The Transportation Department has no objections to the Applicants request for the Master Plan approval provided that the project shall comply with the following:

1. H.C Parking: The proposed parallel H.C parking space is not approved; H.C parking space shall be 17' (12' + 5') wide and shall be relocated as close as possible to the elevator.
2. Bicycle Racks: The owner/developer shall provide a minimum of three (3) inverted U/Hoop bicycle racks near the entrance to each building (total of 12 bicycle parking spaces for the project).
3. Solid Waste: As proposed, approved by Transportation Dept. and Solid Waste Dept.
4. Traffic Control: Show installation of a 36" Stop Signs and 24" Stop Bars at each driveways exit (Stop Sign and Stop Bar shall be in-line and located 4' behind sidewalks/crosswalks) and installation of H.C signs and pavement markings.

Engineering Conditions:

For questions regarding Engineering or Zoning contact Sylvia Johnson at (407)246-3236 or sylvia.johnson@cityoforlando.net

1. The City Council Adopted the Engineering Standards Manual (ESM), Third Edition on January 27, 2003. All plans must conform to the ESM and all construction must be accomplished in accordance to the ESM.
2. Storm water control measures to minimize the impact of the erosion/sedimentation shall be incorporated in the plan of the development for all projects in the City of Orlando and a detailed description of these measures are to be included with the final engineering submittal.

- This is in accordance with Section 6 of the Orlando Urban Storm Water Management Manual.
3. Refuse Container Sites: In accordance with the City Code, Section 28.6 (f) 1-3 the Office of Permitting Services is authorized to make determination of approval/disapproval of refuse container sites. Approval/disapproval of the use of commercial hand pick-up of refuse from any non-residential entity shall be determined solely by the Refuse Collection Bureau Staff.
 4. The owner/developer need to submit a utility plan sanitary sewer and storm drainage for this development.
 5. All sanitary sewer construction is to be in accordance with the Engineering Standards Manual. One lateral for each building is required. Section 9.03.02 (J) of the ESM provides that a double wye is not acceptable.
 6. A landscape plan shall is required to be submitted, pursuant to Chapter 60, Subdivision and Landscaping. All parking lot landscaping and bufferyard requirements, pursuant to Chapter 60, Subdivision and Landscaping, Land Development Code, shall be met.
 7. Please be advised that the development of this project/ property is subject to the terms and provisions of Concurrency Management Chapter 59 of the City Code and the Committed Trip Allocation Policy.
 8. As per Section 61.225 of the Land Development Code, a 5 foot wide concrete sidewalk is required along all dedicated rights-of-way. Any existing sidewalk damaged or broken is to be repaired.
 9. At the time of development, the owner/developer is required to apply an on-site inspection fee that is a percentage of the cost of the on-site improvements, excluding the building, in accordance with City Land Development Code, Section 65.604.
 10. Construction activities including clearing, grading and excavating activities shall obtain an Environmental Protection Agency (EPA) National Pollution Discharge Elimination System (NPDES) permit, except: Operations that result in the disturbance of one acre total land area which are not part of a larger common plan of development or sale.
 11. The owner/developer is required to design and construct an on-site storm water system in accordance with the Orlando Urban Storm Water Management Manual and the approved Master Drainage Plan. Approval from St. John's Water Management District is required. The system is to be privately owned and maintained.
 12. Need to provide drainage calculation for this development. Stormwater Pollution Prevention Plan need to be submitted in accordance with the Florida Department of Environmental Protectio (FDEP) requirement.
 13. The Office of Permitting Services recommends approval of the proposed Master Plan, subject to the above listed conditions and requirements.

Fire Conditions:

For questions regarding Fire contact Doug True at (407)246-2149 or Douglas.True@cityoforlando.net

1. BLDG. 'Leasing' – Identifies a NFPA 13R system going into business occupancy. NFPA 13R systems are strictly for use in residential structures. Provide a NFPA 13 sprinkler system.
2. Vehicle parking area South of Bldg. '207' is 326' long. Provide a turnaround at the end of this dead end or create a path of travel within 150' of the end for vehicles to exit.
3. Vehicle parking area South of Bldg. '206' is 193' long. Provide a turnaround at the end of this dead end or create a path of travel within 150' of the end for vehicles to exit.
4. Site Garden Wall / hedge – Fence obstruction to hydrants, or which create long hose lay distances, or restrict access to Fire Department Connections, will cause comments.



View of Lot 206, where Leasing Office will be located. Slope of site is apparent in this photo taken across New Broad Street.



Existing buildings across from site along Common Way Road



Existing example of accessory structure, for dumpster, or garage or storage. Architectural elements and windows shall match the principal structure.